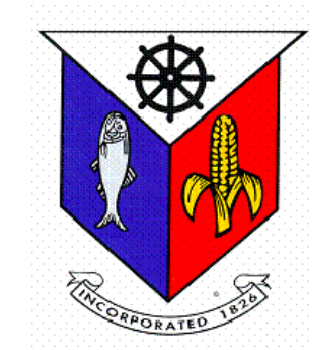


# Wall Street Design Standards



April 16, 2010  
DRAFT

## Overview and Context

Downtown Madison is the heart and soul of the community. Residents and visitors come here regularly to shop, dine, use the post office, attend special events, go to the library, watch a movie, and other activities. A number of people work or live in Madison Center.

It is not only the activities that make Madison Center special. The character, scale, and ambience of Madison Center create and support a sense of place which helps draw people here and makes them feel comfortable. If Madison is going to preserve and enhance its character – a longtime goal of the community – then it must take steps to ensure that appropriate development and redevelopment occur in the Center.

The Advisory Committee on Community Appearance (ACCA) and the Planning and Zoning Commission (PZC) share responsibility for guiding design in Madison Center. Over the years, they have helped guide applicants, promoted design refinements, and coached the community at large in ways to improve and enhance design.

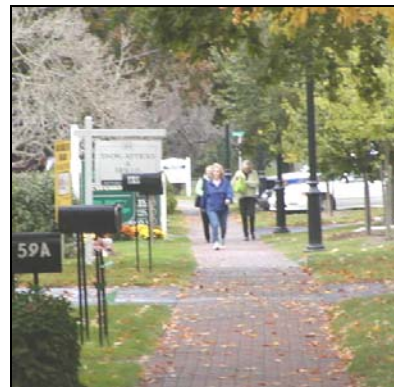
This manual is another step in that journey. Both ACCA and the PZC have found that designers are not always able to create a project that fits into the community on the first attempt. In fact, on some applications, a great deal of time can be devoted to refining a design so that it enhances the overall ambience.

If the “DNA” of Wall Street could be identified and codified, however, this would help property owners and applicants better understand what they can and cannot do with property and provide more confidence regarding what kind of development is appropriate.

This booklet is intended to outline design principles that will be used to guide future activities in the Wall Street corridor in compatible ways. The purpose is to provide guidance so that appropriate development can occur in the future. It will also serve as the basis for amendments to:

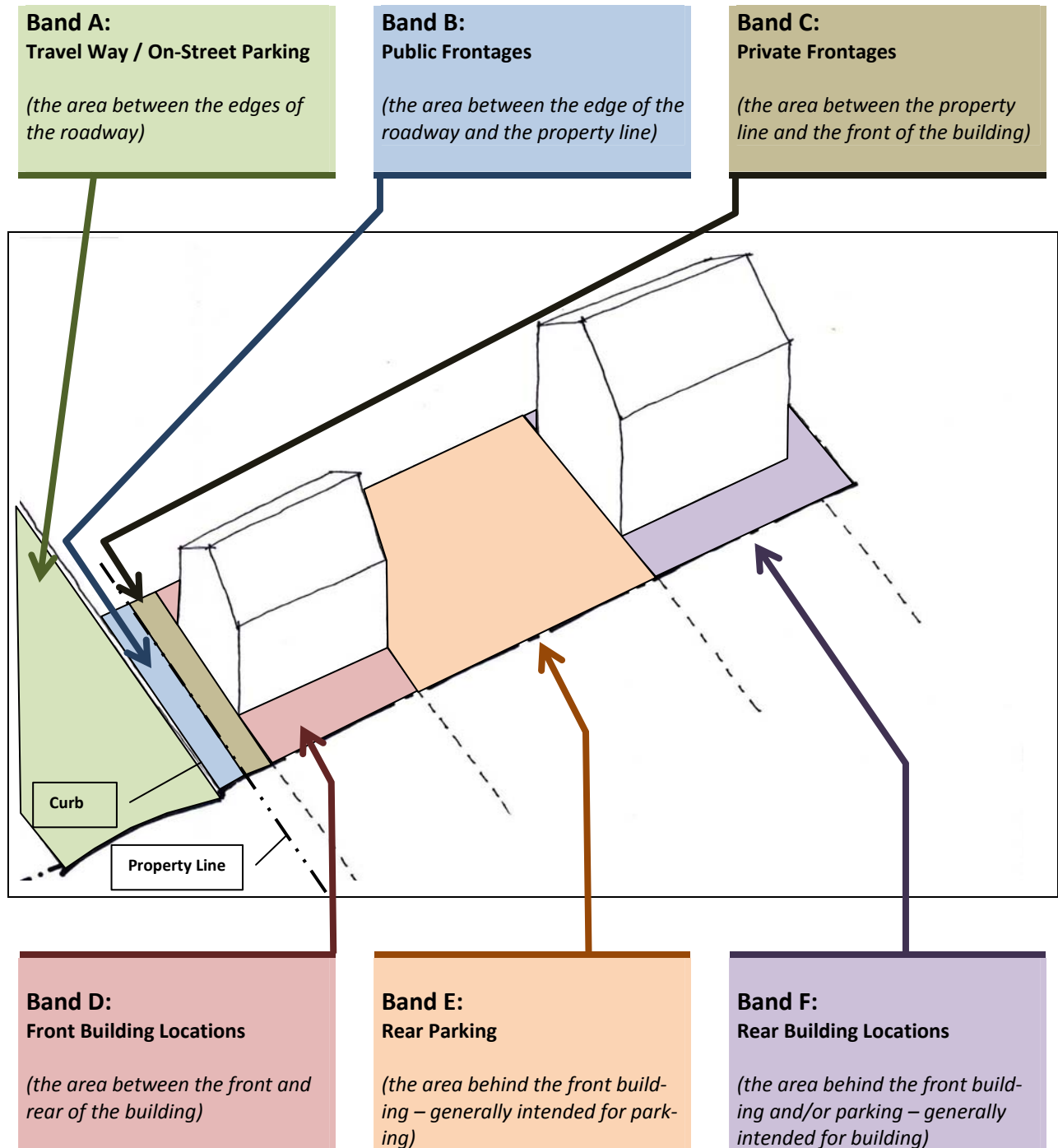
- the regulatory bulk standards contained in Section 6 of the Zoning Regulations, and
- the design standards contained in Section 30 of the Zoning Regulations.

All standards not specifically modified in this booklet (such as signage, parking, etc.) will remain in effect.



## Concept of "Bands"

At the heart of the Standards is the concept of linear "bands". These bands delineate specific areas along the Wall Street corridor - each contributing to the present and desired future character. The band areas are as follows:



## Band A – Vehicular Lanes and On-Street Parking Areas

### CONCEPTUAL GRAPHIC

Band A consists of the paved portions of the road (between the edges of the roadway and/or on-street parking areas)



## Band A – Vehicular Lanes and On-Street Parking Areas

- CONCEPTS –**
- Promote safe circulation patterns within the Wall Street right-of-way
  - Accommodate on street parallel and angle parking
  - Maintain community character
  - Promote pedestrian crosswalks at intersections and other appropriate locations

### Extent

The paved portions of the road right-of-way located between the edges of the roadway and/or on-street parking areas.

### Use Standards

<b>Allowed Uses</b>	Vehicle travel Parking
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### Design Standards

<b>Vehicular Travel</b>	As per Town Road Design Standards
<b>Parking</b>	Combination of diagonal and parallel parking
<b>Parking - East Side</b>	Primarily parallel parking spaces <ul style="list-style-type: none"> <li>• 8 feet wide</li> <li>• 22 feet long</li> </ul>
<b>Parking - West Side</b>	Primarily diagonal parking <ul style="list-style-type: none"> <li>• Head-in parking</li> <li>• 60° angle (approx.)</li> <li>• 9 feet wide</li> <li>• 18 feet deep</li> </ul> Some parallel parking <ul style="list-style-type: none"> <li>• see above</li> </ul>
<b>Curb Material</b>	Raised granite
<b>Drainage</b>	Catch basins

Band A includes the on-street travel-way and parking areas. This band is an important part of the public realm and is important to maintaining design continuity in the corridor.

It is important to consider design principles and standards for this area since on-street parking, driveway location, or other activities might be proposed that could affect this area.

## Band B – Public Frontages

### CONCEPTUAL GRAPHIC

Band B – Public frontage consists of the area between the paved portion of the road and the property line



## Band B – Public Frontages

- CONCEPTS –**
- Promote pedestrian circulation
  - Provide landscape and streetscape amenities
  - Provide for a transition from public to private uses

### Extent

The area between the paved portion of the road right-of-way and the property line

### Use Standards

<b>Allowed Uses</b>	Pedestrian circulation Private driveways, where approved Landscaping Street and directional signage Public amenities Public utilities
<b>Prohibited Uses</b>	Private signage

### Design Standards

<b>Landscaped Areas</b>	Grass strips (generally 2 – 6 feet wide) between curb / sidewalk and sidewalk / property line
<b>Canopy Trees</b>	See Appendix for listing of approved street tree species
<b>Walkway</b>	Public sidewalk <ul style="list-style-type: none"> <li>• Parallel to street</li> <li>• 5 – 8 feet wide</li> <li>• Pavers or concrete</li> <li>• At driveways, the walking surface shall be continuous through the driveway surface unless modified by the Planning and Zoning Commission</li> </ul>
<b>Public lighting</b>	Street lighting (pole, fixture, placement, etc.) shall match existing
<b>Utilities</b>	Underground preferred

## Band C – Private Frontages

### CONCEPTUAL GRAPHIC

Band C – Private Frontage consists of the area between the property line and the building wall (or build-to-line)



## Band C –Private Frontages

- CONCEPTS –**
- Promote pedestrian circulation
  - Provide landscape and streetscape amenities
  - Provide for a transition from public to private uses

### Extent - Private Frontage

The area between the property line and the building wall (or build-to-line)

### Use Standards

<b>Allowed Uses</b>	Pedestrian circulation Private driveway, where approved Landscaping Private signage Site amenities Porches / stoops
<b>Discouraged Uses</b>	Fencing Hedges Stonewalls
<b>Prohibited Uses</b>	Private parking Mechanical equipment Service areas (refuse, recycling, loading, etc.)

### Design Standards

<b>Landscaped Areas</b>	Lawn and other landscaped treatment between façade and front property line.
<b>Canopy Trees</b>	Approved species to complement street trees (see Appendix for list of approved street tree species)
<b>Walkway</b>	Private walkway <ul style="list-style-type: none"> <li>• Primarily perpendicular to street</li> <li>• 4 feet wide minimum</li> <li>• Pavers or concrete</li> <li>• At driveways, the walking surface shall be continuous through the driveway surface unless modified by the Planning and Zoning Commission</li> </ul>
<b>Private lighting</b>	Fixture that is complementary to street light fixtures and buildings in terms of style, finish, character, and level of illumination
<b>Utilities</b>	Underground strongly preferred

## Band D – Street Facades and Buildings

### CONCEPTUAL GRAPHIC

Band D consists of the area between the front and rear facades of buildings



## Band D – Street Facades and Buildings

- CONCEPTS –**
- Encourage residential-scale buildings oriented to an active streetscape
  - Preserve existing buildings and encourage any additional development to be similar in scale and character
  - Minimize the number of access drives to rear parking areas and share access drives

### Extent

The area on the lot defined by (and between) the façade (front wall) and rear wall of the principal building extending from one side property line to the opposite side property line

### Use Standards

- |                         |  |
|-------------------------|--|
| <b>First Floor Uses</b> | Retail (preferred)<br>Restaurant (allowed)<br>Office (discouraged)   |
| <b>Upper Floor Uses</b> | Office (encouraged)<br>Residential (encouraged)  |
| <b>Other Uses</b>       | Parking is prohibited in Band D except that the Planning and Zoning Commission may allow such parking when it: <ul style="list-style-type: none"> <li>• will enhance the overall parking supply for all properties, and</li> <li>• does not detract from the streetscape.</li> </ul> |

### Design Standards

<b>Building Location</b>	Minimum	10 feet
<b>– Front Setback</b>	Maximum	20 feet
<b>Building Location</b>	Side Yard –	6 feet minimum
<b>– Other Setbacks</b>		20 feet total
	Rear Yard –	15 feet
<b>Building Location</b>	Buildings should generally have the long dimension perpendicular to the street.	
<b>- Orientation</b>		

*(continued)*



**Design Standards (continued)**

**Building Scale** Modest size buildings, similar in size and scale to existing buildings, are strongly preferred

**Building Scale - Footprint** Building footprints shall not exceed 2,000 SF however, with significant articulation and variety of form and massing, the Planning and Zoning Commission may allow a building with a footprint of up to 2,500 SF

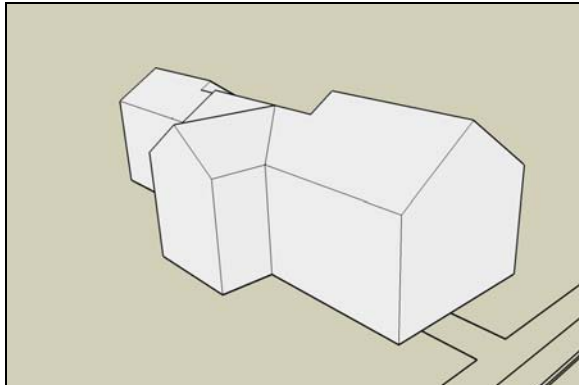
**Building Shape** Buildings should be articulated and varied in form and massing

**Design Standards (continued)**

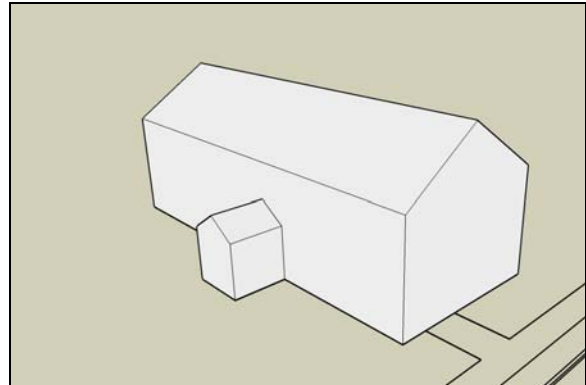
**Building Width** Buildings fronting on Wall Street shall have a maximum façade width of 30 feet

Total width of the building may be wider provided that any secondary front elevation is located behind a 45-degree angle from the façade (see graphic)

**Building Scale / Footprint**

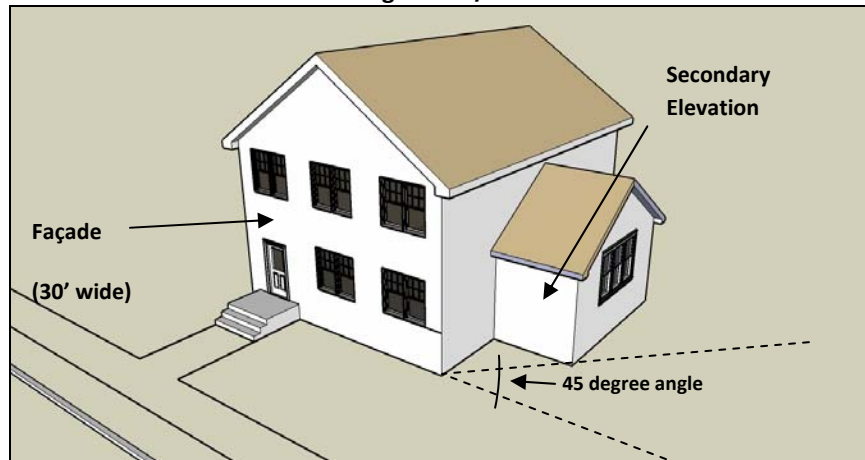


An example with significant building articulation and variety of form and massing (2,350 SF footprint).



An example without significant building articulation or variety of form and massing (2,350 SF footprint).

**Building Width / Setback**



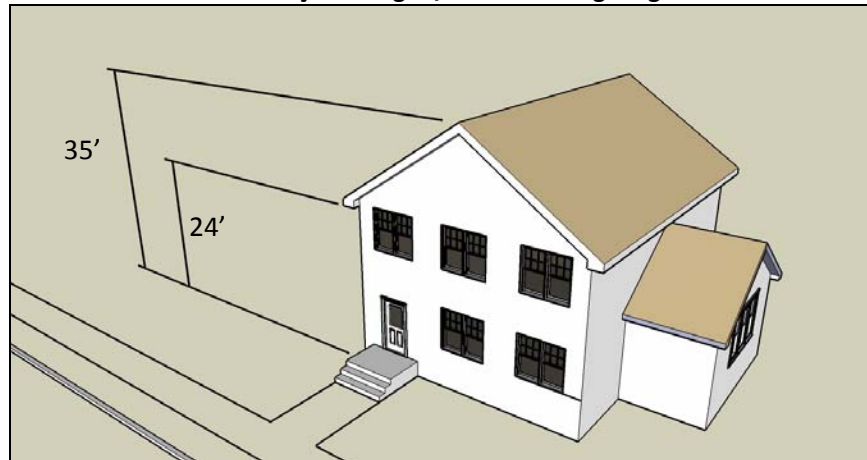
While the façade is 30 feet wide, additional building width is allowed since the secondary elevation is located behind a 45-degree angle drawn from the façade.

The building footprint must still be less than the 2,000 SF maximum.

Design Standards (continued)	
<b>Street Façade Height</b>	Buildings fronting on Wall Street shall have a minimum street facade height of 18 feet and a maximum street facade height of 24 feet measured from average grade to the underside of the eave or to the top of the parapet
<b>Total Building Height – Feet</b>	35 feet maximum to highest point of roof
<b>Total Building Height – Stories</b>	Minimum - 1.5 stories Maximum - 2.5 stories

Design Standards (continued)	
<b>Building Materials</b>	Wood (strongly encouraged)  Brick (allowed) Composite (allowed)  Vinyl siding (discouraged) Stone (discouraged except for foundation)  Stucco (strongly discouraged) Aluminum (strongly discouraged) EIF system (strongly discouraged) Curtain wall (strongly discouraged) Panel wall (strongly discouraged) Block masonry (strongly discouraged)
<b>Mechanical Equipment and Service Areas</b>	Mechanical equipment, utility services, and other service areas (refuse, recycling, loading, etc.) may be located within Band D provided such areas are effectively screened and the overall design and location has been accepted by ACCA and approved by the Planning and Zoning Commission

Street Façade Height / Total Building Height



This building has:

- a street façade height of 24 feet (measured to the underside of the eave) and between 18' and 24' is required.
- a total building height of 35' (measured to the highest point of the roof) which is the maximum allowed.

## Band E – Rear Parking / Service Area

CONCEPTUAL GRAPHIC

Band E consists of the area behind the rear elevations of buildings



## Band E – Rear Parking / Service Area

- CONCEPTS –**
- Encourage parking in the rear to serve uses
  - Interconnect parking on all lots in order to enhance the parking lot design and to help meet the peak parking demands of different uses
  - Interconnect parking behind the front building to serve multiple uses

### Extent

The area behind the rear elevation of buildings located in Band D

### Use Standards

**Allowed Uses**      Parking  
                                  Interconnecting drives and parking areas  
                                  Building service areas

**Prohibited Uses**   Buildings that will prevent interconnections between properties on the same side of the street

### Design Standards

**Parking Location**   Parking shall generally be provided behind the rear elevation of the building fronting on Wall Street

Discourage closed or “dead-end” lanes

Encourage landscape islands and stormwater infiltration

**Access**                      Interconnections of parking lots and sharing of driveways is strongly recommended and may be required by the Planning and Zoning Commission

**Mechanical Equipment and Service Areas**   Mechanical equipment, utility services, and other service areas (refuse, recycling, loading, etc.) may be located within Band D provided such areas are effectively screened and the overall design and location has been accepted by ACCA and approved by the Planning and Zoning Commission

## Band F – Rear Facades and Buildings (West Side)

CONCEPTUAL GRAPHIC



## Band F – Rear Facades and Buildings (West Side)

- CONCEPTS –**
- Encourage rear buildings oriented to rear parking areas and drives when provision has been made for vehicular interconnections between sites
  - Encourage new and infill development of appropriate scale and character
  - Minimize access drives to rear parking areas and share access drives

### Extent - West

The area west of the rear parking areas (Band E) to Tuxis Pond and associated wetlands

### Use Standards - West

<b>First Floor Uses</b>	Lodging (encouraged) Office (encouraged) Retail (allowed) Restaurant (allowed) Residential (discouraged)
<b>Upper Floor Uses</b>	Lodging (encouraged) Office (encouraged) Residential (encouraged)
<b>Allowed Uses</b>	Parking

### Design Standards - West

<b>Building Location –Setbacks</b>	Side Yard – 6 feet minimum Rear Yard – 20 feet total 15 feet
<b>Building Scale - Footprint</b>	Building footprints shall not exceed 3,000 SF
<b>Building Shape</b>	Buildings should be articulated and varied in form and massing
<b>Total Building Height – Feet</b>	35 feet maximum to highest point of roof
<b>Total Building Height – Stories</b>	Maximum – 2.5 stories
<b>Building Materials</b>	Wood (strongly encouraged)  Brick (allowed) Composite (allowed)  Vinyl siding (discouraged) Stone (discouraged except for foundations)  Stucco (strongly discouraged) Aluminum (strongly discouraged) EIF system (strongly discouraged) Curtain wall (strongly discouraged) Panel wall (strongly discouraged) Block masonry (strongly discouraged)

## Band F – Rear Facades and Buildings (East Side)

### CONCEPTUAL GRAPHIC

Band F consists of the area behind the rear elevations of Band D buildings (only the east side of Wall Street is shown in this graphic)



## Band F – Rear Facades and Buildings (East Side)

- CONCEPTS –**
- Encourage rear buildings oriented to rear parking areas and drives when provision has been made for vehicular interconnections between sites
  - Encourage new and infill development of appropriate scale and character
  - Larger size buildings may be allowed with an appropriate transition to adjacent uses
  - Minimize access drives to rear parking areas and share access drives

### Extent - East

The area east of the rear parking areas (Band E) to the rear property lines (or zone boundary)

### Use Standards - East

<b>First Floor Uses</b>	Office (encouraged) Retail (allowed) Residential (discouraged)
<b>Upper Floor Uses</b>	Residential (encouraged) Office (allowed)
<b>Allowed Uses</b>	Parking

### Design Standards - East

<b>Building Location –Setbacks</b>	Side Yard –	6 feet minimum
		20 feet total
	Rear Yard –	15 feet
<b>Building Scale - Footprint</b>	Building footprints shall not exceed 3,000 SF however, with significant articulation and variety of forma and massing, the Planning and Zoning Commission may allow a building with a footprint of up to 3,500 SF	
<b>Building Shape</b>	Buildings should be articulated and varied in form and massing	

### Design Standards – East (continued)

<b>Total Building Height – Feet</b>	35 feet maximum to the highest point of the roof
	Total building heights up to 45 feet may be permitted by the Planning and Zoning Commission when viable vehicle interconnections are provided for between properties
<b>Total Building Height – Stories</b>	Maximum – 2.5 stories
	3.5 stories when permanent vehicle interconnections between properties are provided for
<b>Building Materials</b>	Wood (strongly encouraged)
	Brick (allowed)
	Composite (allowed)
	Vinyl siding (discouraged)
	Stone (discouraged except for foundations)
	Stucco (strongly discouraged)
	Aluminum (strongly discouraged)
	EIF system (strongly discouraged)
	Curtain wall (strongly discouraged)
	Panel wall (strongly discouraged)
	Block masonry (strongly discouraged)

